 <b>Reigate &amp; Banstead</b> <b>BOROUGH COUNCIL</b> Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	1 <sup>st</sup> August 2018
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
	<b>AUTHOR:</b>	Rosie Baker
	<b>TELEPHONE:</b>	01737 276173
	<b>EMAIL:</b>	rosie.baker@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	7	<b>WARD:</b> South Park and Woodhatch

<b>APPLICATION NUMBER:</b>	18/00916/F	<b>VALID:</b>	3/5/2018
<b>APPLICANT:</b>	Earlswood Homes	<b>AGENT:</b>	White & Sons
<b>LOCATION:</b>	<b>106 DOVERS GREEN ROAD AND REAR OF 104 DOVERS GREEN ROAD, REIGATE, SURREY</b>		
<b>DESCRIPTION:</b>	<b>The demolition of No 106 Dovers Green Road and erection of 5 x 5 bed dwellings with associated access, parking and landscaping. As amended on 07/06/2018 As amended on 11/6/2018</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the construction of five detached, five bedroom houses, with accommodation over three floors and integral garaging. The site comprises the entire of 106 Dovers Green Road and part of the rear garden of no.104. It is proposed to create a new shared access to Dovers Green Road for plots 1 and 2; three new private driveways to Beehive Way are proposed for units 3, 4 and 5. The site is situated in a sustainable location within the urban area. A number of individual trees on site are protected by Tree Preservation Order Ref: RE69.

This application follows a recently refused scheme for seven (reduced to six) dwellings that was refused solely on impact to character given its high density, cramped layout with small plot sizes and bulky building forms. This application is considered to overcome these issues with a revised design approach that includes a reduction in number of dwellings, a more spacious layout and a traditional detached design form of improved quality.

There is no in principle objection to the redevelopment of the site and it is noted the existing site is very low density in comparison to the surrounding development, particularly the more recent housing additions in Beehive Way. A density assessment supports the application which confirms the proposed density (17 dph) is similar to that of the established pattern of development in Dovers West and Dovers Green Road (10 – 19dph) and lower than the newer forms of development in Beehive Way and the estate to the west beyond (20 – 29 dph). It is considered that

the redevelopment of the site will make efficient use of land whilst respecting the existing character and density of the locality. It is noted predominantly two storey built forms characterise the area. Whilst dormers are proposed, with the scheme incorporating habitable accommodation within the roof space, I consider this acceptable noting that all the dormers face towards the rear and therefore will have a negligible impact on the streetscene. Conditions are proposed to control future extensions.

The proposed detached houses are of traditional design and subject to conditions regarding materials and an acceptable landscape scheme the design and layout is considered acceptable. The proposed dwellings would be appropriately spaced from each other and their boundaries, with available space to provide a meaningful landscape scheme. Their relationship and distances to neighbouring properties would prevent any harmful impact upon neighbouring amenity. The tree officer has assessed the application and confirmed subject to condition the development would have an acceptable impact on trees.

Concern has been raised by residents regarding the introduction of new access onto Beehive Way. The highway impacts of the development have been assessed by the County Highway Authority and have been deemed acceptable. Concern has also been raised regarding inconvenience during construction. A construction method statement is proposed to be conditioned.

## **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.

## **Consultations:**

Highway Authority: No objection subject to condition.

The County Highway Authority has assessed the application on safety, capacity and policy grounds and is satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway with respect of access, net additional traffic generation and parking. The County Highway Authority therefore has no highway requirements subject to conditions

Surrey County Council (Minerals and waste): No objection

Surrey County Council (SuDs): Application beneath threshold no comment

Reigate Society: Preference for one dwelling on the Dovers Green Road frontage and question the need for 5 bedroom dwellings.

Tree officer: No objection subject to conditions (see report)

## **Representations:**

Letters were sent to neighbouring properties on 14<sup>th</sup> May 2018 and a site notice was posted 6<sup>th</sup> June 2018. Neighbours were re-notified on the revised plans for a 14 day period commencing 12<sup>th</sup> June 2016.

5 responses have been received raising the following issues. This includes a letter from the residents of nos 1, 4, 6, 10 and 12 Beehive Way

<b>Issue</b>	<b>Response</b>
Introduction of new vehicular accesses to Beehive Way – Preference for all dwellings to be accessed from Dovers Green Way	See paragraph 6.22 – 6.24
Increase in traffic and congestion, in particular with respect to Beehive Way	See paragraph 6.22 – 6.24
Hazard to highway safety, in particular with respect to Beehive Way	See paragraph 6.22 – 6.24
Lack of parking on Beehive Way	See paragraph 6.22 – 6.24
Inconvenience during construction, in particular use of Beehive Way by construction vehicles.	See paragraph 6.20
Out of character with surrounding area, (concern regarding use of dormers to achieve accommodation in the roof, scale / width of development proposed at plot 5, spacing of dwellings)	See paragraph 6.4 – 6.12

Poor design	See paragraph 6.4 – 6.12
Overlooking, overshadowing and loss of privacy	See paragraph 6.16 – 6.21
Alternative location / proposal preferred	Each application must be assessed on its own merits

## **1.0 Site and Character Appraisal**

- 1.1 The site is located in a predominantly residential area on the western side of Dovers Green Road, a busy A road linking Reigate to Sidlow. The site comprises the entire of 106 Dovers Green Road and part of the rear garden of no.104. The site is occupied by a single large two storey detached house set in a generous plot. There are protected trees in the rear garden of both 104 and no 106 and the surrounding area is characterised by a high level of tree cover, apart from the southern boundary which is quite open to Dovers West separated by a small hedge
- 1.2 Buildings here are generally well spaced, two stories and of detached form, considerably set back from the highway along a well-established building line. The site is currently accessed from Dovers West a small private road to the south of the site, providing access to 6 detached dwellings: 1-4 Dovers West a small recent development of 4 detached homes, 108 a bungalow and 108a a two storey home. To the north of the site is Beehive Way (itself an infill development), which consists of predominantly two storey detached houses. The site is relatively flat.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: The opportunity did not arise because the applicant did not approach the Local Planning Authority before submitting the application.
- 2.2 Improvements secured during the course of the application: A revised layout was submitted improving the separation distances between units 1 and 2 and 3 and 4. Given the presence of protected trees in the proximity of unit 4 revised arboricultural information was submitted to demonstrate the revised layout would not have a detrimental impact on protected trees.
- 2.3 Further improvements are proposed to be secured through the use of conditions.

## **3.0 Relevant Planning and Enforcement History**

- |     |            |   |  |
|-----|------------|---|--|
| 3.1 | 17/02741/F | Demolition of No.106 and erection of 2 x 3 bed, 4 x 4 bed and 1 x 5 bed dwellings with associated access, parking and landscaping | Refused<br>Appeal awaiting determination |
|-----|------------|---|--|

3.2	13/00081/TPO	Fell one horse chestnut infected with honey fungus	Granted
3.3	83P/1049/F	106 Dovers Green Road, Two storey side extension and attached double garage	Approved
3.4	82P/0417/O	106 Dovers Green Road. Erection of 2 detached dwellings with attached garages (renewal of planning permission 79p/0197).	Refused. Appeal dismissed on tree grounds.
3.5	80P/1265	Application in outline. Erection of one detached house between the two existing dwellings at nos 104 – 106 Dovers Green Road and formation of a new access to dovers green road	Approved
3.6	80P /1264	Erection of 6 detached houses fronting beehive way, part curtilage of 102, 104 and 106 Dovers Green Road.	Refused. Appeal dismissed.
3.7	80P/0301	Erection of 7 detached houses on land between 104 and 106 Dovers Green Road and fronting beehive way	Refused. Appeal dismissed.
3.8	79P/1102	Erection of 6 detached houses with service road and access to beehive way – land to the rear of 102 – 106 Dovers Green Road	Refused. Appeal dismissed.
3.9	79p/0197/O	Erection of two detached dwellings with attached garages (renewal of planning reference re.75p/312)	Approved with conditions.
3.10	75p/312/O	Erection of two detached dwellings with attached garages between the two dwellings of 104 – 106	Refused. Allowed on appeal
3.11	74p/0408	Erection of 9 two storey detached dwellings and garages and construction of estate road with access to beehive way	Refused
3.12	74/0223	Demolition of 104 Dovers Green Road and erection of 9 two storey detached dwelling houses and garage and construction of estate road with access to beehive way	Refused

#### 4.0 Proposal and Design Approach

- 4.1 This is a full application for the demolition of 106 Dovers Green Road, the subdivision of the plot and the erection of 5 no 5 bed dwellings, together with associated parking and landscape works. It is proposed to create a new shared access to Dovers Green Road for plots 1 and 2. Three new private driveways to Beehive Way are proposed for units 3, 4 and 5. Plots 1 -4 have an integrated garage space, plot 5 has an attached singled storey garage to the side. Additional external parking is provided within private driveways.
- 4.2 The application follows a scheme for the erection of seven dwellings, reduced to six following amendment to the application recently refused on character grounds relating to the overdevelopment of the site, proposed design and failing to respect the character of the area. As set out in the planning history there have also been a number of historic applications for the subdivision of the plot to provide for 9, 7 and 6 dwellings respectively all of which were refused and a number dismissed on appeal in the 1970s and 1980s. An application for the erection of one detached house on land between 104 and 106 was approved in 1980. An outline application for two detached dwellings between 104 and 106 was approved in the 1970s, reduced to one dwelling given arboricultural interests (TPO served) in 1980.
- 4.3 The current proposal seeks to overcome the previous reason for refusal by providing fewer dwellings (from 6 to 5), a revised design of the dwellings (removing the awkward and bulky barn roofs) and greater space between buildings. The layout has been revised and the applicant has amended the access arrangements such that 3 of the units are now proposed with private driveways to Beehive Way. All dwellings are now proposed as detached. As previously the layout has been arranged with two units fronting Dovers Green Road. Plots 3 – 5 would be sited to the rear and now face Beehive Way, with plot 5 within the rear garden of 104 Dovers Green Road.
- 4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.5 Evidence of the applicant's design approach is set out below:

Assessment	The character of the surrounding area is assessed as residential of varied density. The host dwelling forms part of the estate built mainly during the 1930s and 1950s, as identified within the Council's Distinctiveness Guide (page 22). As such the housing locally comprises of medium to
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	large detached dwellings set back from the Dovers Green Road. The estate to the east is typically higher in density with the newer additions in the area providing an even higher density.
	Site features meriting retention are listed as protected trees.
Involvement	No community consultation took place.
Evaluation	The proposed design responds to the previous refused scheme for 6 units
Design	The traditional design approach is considered to respect the character of the area and is reflective of the Surrey vernacular in general. The proposed scale is considered comparable to those dwellings that predominate and responds to the general density and pattern of development in the locality. The applicant has provided an assessment of densities in the local area to support their position.

#### 4.6 Further details of the development are as follows:

Site area	0.29ha
Existing parking spaces	4
Proposed parking spaces	15
Parking standard	2 spaces per dwelling (3 bedroom plus)
Net increase in dwellings	4
Existing site density	3.44 dph
Proposed site density	17 dph
Density of the surrounding area	25.5 dph 4 – 14 Beehive Way 18.6 dph 1,1A, 3 Beehive Way & 100-102 Dovers Green Road 13.56 dph Dovers West 13.64 dph 108 – 122 Dovers Green Road

## 5.0 Policy Context

### 5.1 Designation

Urban area  
Tree Protection Order (RE69)

## 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS4 (Valued Townscapes and the Historic Environment)  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),  
CS14 (Housing Needs)  
CS15 (Affordable Housing)  
CS17 (Travel Options and accessibility)

## 5.3 Reigate & Banstead Borough Local Plan 2005

Landscape & Nature Conservation	Pc4
Housing	Ho9, Ho13, Ho14, Ho16,
Movement	Mo5, Mo7,

## 5.4 Other Material Considerations

National Planning Policy Framework	
National Planning Practice Guidance	
Supplementary Planning Guidance	Surrey Design Local Distinctiveness Design Guide A Parking Strategy for Surrey Parking Standards for Development Affordable Housing
Other	Human Rights Act 1998 Community Infrastructure Levy Regulations 2010

## 6.0 **Assessment**

- 6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms.
- 6.2 Whilst there have been previous refusals on this site they related to a higher number of dwellings and materially different site layout. The site is sustainably located on the edge of Woodhatch. There is no objection in principle to a potential redevelopment of the site and such a redevelopment would help the Council meet some of the Borough's identified housing need and furthermore would be welcomed as a contribution to housing supply, with associated employment and economic benefits. However, the assessment of acceptability in this case rests upon considering the impact of the proposal and resultant harm and the need to provide additional housing, and its resultant benefit.
- 6.3 The main issues to consider are:
- Design appraisal



- Neighbour amenity
- Highway matters
- Affordable Housing
- Community Infrastructure Levy

Design appraisal

- 6.4 This application follows a recently refused scheme for seven (reduced to six) houses that was refused as the proposed design and layout (specifically the number of dwellings, bulky building forms, uncharacteristically small plots, lack of spaciousness and poor layout of the access road and parking areas with limited space for landscaping) was considered to result in a cramped overdevelopment of the site and harmful to the character of the area. This scheme seeks to overcome this previous reason for refusal with a reduced number of dwellings and a revised design approach.
- 6.5 Policy Ho13 of the local plan states that maintenance of the character should be of prime consideration and is of particular relevance for infill development. Local plan policy Ho14 relates specifically to back land development. It states that such development must comply with a number of criteria. Including that the general pattern and form of development in the area is maintained, that proposed plot sizes and spacing between buildings reflect that predominating within the surrounding area and that new access roads do not create undue disruption to the character and appearance of the existing road frontage. These design and character policy principles are also reiterated in the Council's Local Distinctiveness Guide, particularly with reference to Case Study 3 which provides design guidance for infill development.
- 6.6 The revised application for 5 units is considered to overcome previous concerns regarding density and the overdevelopment of the site. Amendments during the course of the application has further increased the separation distances between the dwellings, such that they are now considered satisfactory. There is a gap of 4m between plots 1 and 2. To the rear the gaps are approx 4.2m between plots 3 and 4 and approx 13m between plots 4 and 5. Building to building there is a gap of approx 21m between the rear building line of plots 1 and 2, and dwellings 3 and 4. The separation distance is approx 24m between 104 Dovers Green Road and dwelling 5.
- 6.7 The proposal will achieve a density of 17 dwellings per hectare. The applicant has provided a density assessment mapping the density of the local area, shown at page 12 of the submitted design and access statement. It is clear from this assessment, and I have undertaken my own shown in the table at 4.5 above that the proposed density is not dissimilar to that of the established pattern of development in Dovers West and Dovers Green Road and lower than the newer forms of development in Beehive Way and the estate to the west beyond. The existing site and the adjacent property of 104 Dovers Green Road in density terms is significantly lower than the surrounding built form and it is considered that the redevelopment of the site will make efficient use of land whilst respecting the existing character and density of the locality.

- 6.8 The proposed layout of the development will correspond to established building lines along Dovers Green Road and Beehive Way, with appropriate set backs from the roads and allowing appropriate space for front gardens, driveways and parking. The applicant's 'massing plan' illustrates the building footprints of the development in comparison to those within the locality. This indicates that detached forms are predominant in the locality and the proposal would broadly conform to the existing pattern of development. Whilst it is noted plot 5 is larger (wider) than most, it is noted there are significant properties on Dovers Green Road including semi-detached forms of similar width. Whilst plot 5 is detached I do not consider plot 5 so out of character (noting its spacious plot) to warrant refusal.
- 6.9 The proposed dwellings are two storeys in scale with accommodation provided in the roof and dormers incorporated to the rear. The proposed buildings heights will not exceed 9m which is comparable to other dwellings in the locality. The applicant states that the built form will be lower than that proposed under the previous application 17/02741/P with no significant change to existing site levels. Given the scale of the dwellings the roofs would be capable of conversion at a future date. Whilst adding to the bulk and mass of the roof, the dormers are all located to the rear and therefore would have a limited impact on the appearance of the streetscene. The dormers proposed are all of the same size and profile and accord with the Council's Supplementary Planning Guidance on this matter. As such I consider limited harm arises from their presence and would be insufficient to result in a reason for refusal on this basis.
- 6.10 The dwellings are traditional in design, finished in brick with the first floor tile hung with timber overhang porches and brickwork details to add visual interest. Whilst there is consistency with respect to the palette of materials and general design there is some variation in houses sizes and types which is welcomed and adds variety to the streetscene. The proposed materials and designs are considered to accord with the Surrey vernacular and are considered appropriate in this location.
- 6.11 Part 3 of the Local Distinctiveness Guide out a series of design considerations for 1930s and 1950s character areas to which this site relates. The guide provides as follows: "Consideration should be given to the materials existing within the immediate vicinity as a means of integrating new development. New infill development should not provide parking or garaging which dominates the street frontage. Building lines should be respected, particularly where this is a dominant feature. Visual separation between dwellings should be retained. Building form and massing should reflect dwellings within the vicinity."
- 6.12 Subject to securing appropriate materials and an acceptable landscape scheme as noted below the dwellings would sit comfortably within the site and its surroundings and the revised design approach is considered to overcome the previous reason for refusal relating to character. I consider the

proposal to conform with the guidance and the criteria of local plan policies Ho13 and Ho14.

- 6.13 The tree officer was consulted on the proposal in order to assess the proposed development against impact upon existing trees and vegetation. No objection has been raised and subject to conditions relating to landscaping and tree protection.
- 6.14 The detailed comments of the tree officer are set out below:

*“Rosie following our recent discussion, I have reviewed amended plans and the revised arboricultural information which has been prepared by David Archer Associates. The information has been compiled in accordance with the guidelines advice and recommendation contained within British Standard 5837.*

*Subject to adherence to the prescribed tree protection measures and arboricultural method statement, which makes provision for pre commencement meeting, qualified arboricultural supervision at the relevant stages of development and the ongoing qualified monitoring and reporting process there should be no significant or long lasting adverse or impact on the future health and vigour of the trees being retained on the proposed development site. Trees lost directly to the proposal are of a domestic nature; trees have been categorised in the lower categories contained within table 1 and the criteria detailed in section 4 of the above standard.*

*The arboricultural matters relating to this site can therefore be adequately dealt with by compliance condition which is attached. I have also attached a landscape conditions which requires replacement tree planting, whilst space is limited on site there is available space to improve and enhance the existing and future landscape of this locality, which should be secure by condition.”*

- 6.15 Whilst it is noted that the application will result in the loss of some trees these are of a domestic nature. The three most significant trees are the protected scots pine numbered T11, T13 and T14 in the submitted survey, one category A and two category B trees, and are shown for retention. These trees are significant in the local and wider landscape and afford high visual amenity to the general area. Whilst the proposed development involves some minor incursions into the calculated root protection areas of retained trees these incursions are nominal and can be managed by condition. In light of the above and subject to condition the proposal is considered to comply with policy Pc4 of the Local Plan.

#### Neighbour amenity

- 6.16 Due to the design, siting and aspect between the dwellings proposed and neighbouring properties, no direct loss of privacy or light would occur and the proposed dwellings would be sufficiently distanced from neighbouring properties as to not result in harmful overbearing presence or unacceptable overshadowing. Whilst resulting in a degree of greater presence, overlooking

and change in outlook the impact would not be significant to justify a refusal in his case. Separation distances (wall to wall) are provided below and are considered acceptable noting the closest relationship is between Plot 5 and 3 Beehive Way and reflects a proposed flank elevation with side garage to existing blank flank elevation relationship.

Plot 1 to 104 Dovers Green Road: 13m  
Plot 5 to 104 Dovers Green Road: 24m  
Plot 5 to 3 Beehive Way: 6.8m (4m to garage)  
Plot 5 to 10 - 6 Beehive Way: 23.5 - 25m  
Plots 3 – 4 to 14 – 12 Beehive Way: 29.7 - 32m  
Plot 3 to Dovers West: 23 – 28m  
Plot 3 to 108 Dovers Green Road: 23m  
Plot 2 to 108a Dovers Green Road: 25m

- 6.17 It is noted that 108 is a bungalow and the proposal relates to two storey development with roof accommodation, notwithstanding this the separation distance, which includes the internal road of Dovers West between the properties is considered acceptable and there would be no harmful impact. The current boundary along Dovers West is relatively open and a 1.8m boundary fence is proposed which would provide improved privacy. Boundary treatment and additional planting would be secured by condition.
- 6.18 First floor bathroom windows can be conditioned to be obscure glazed and fixed shut except for a top hung fan light and those serving ancillary spaces such as staircases and corridors are not considered to give rise to harm by reason of overlooking or loss of privacy. No other windows at first floor are proposed in the building flanks. In light of the separation distances the first floor bedroom windows to the rear and front are not considered to give rise to amenity harm to existing residents. Future residents would be aware of the building relationships and level of overlooking to rear gardens prior to purchase and as such I consider the level of harm very limited. The dormers are all to the rear and as such are not considered to result in an additional level of harm beyond that which would already exist from the first floor windows. In light of the above the development would not result in a harmful loss of privacy to neighbouring properties.
- 6.19 Living standards: The proposed dwellings in terms of their layout, size, accessibility and access to facilities is considered acceptable. The proposed dwellings have an acceptable floor space and the units would have access to private amenity space. When judged from a living standard perspective the proposal is considered acceptable.
- 6.20 Objection was raised on the grounds of inconvenience during the construction period. Whilst it is acknowledged there may be a degree of disruption during the construction phase, the proposal would not warrant refusal on this basis and statutory nuisance legislation exists to control any significant disturbance caused during the construction of the proposal. A construction method statement would be secured by planning condition.

- 6.21 While giving rise to a degree of change in the relationship between buildings, the proposed scheme would not adversely affect the amenity of neighbouring properties, and complies with policy Ho9, Ho13 and Ho14 in this regard.

Highway matters

- 6.22 A new shared access is proposed from Dovers Green Lane for plots 1 and 2. In addition three new private drives from Beehive Way are proposed for plots 3, 4 and 5. A total of 15 parking spaces are proposed (including single integral garages to plots 1 -4 and an attached single garage to plot 5).
- 6.23 The County Highway Authority having considered local representations has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision. It is satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway including Beehive Way subject to conditions relating to the accesses and the requirement for a construction transport management plan. Conditions to secure the parking layout prior to occupation are also considered appropriate.
- 6.24 Parking provision accords with adopted standards and on this basis is considered acceptable. In light of the above there is no objection to the scheme from a highway perspective and the proposal is considered to accord with the provisions of the NPPF, and Local Plan policies Mo5, Mo6 and Mo7 in this respect.

Affordable Housing

- 6.25 The development would result in a net gain of four residential units. Core Strategy Policy CS15 and the Council's Affordable Housing SPD require financial contributions towards affordable housing to be provided on housing developments of 1-9 units. However, in November 2014, the Government introduced policy changes through a Written Ministerial Statement and changes to the national Planning Practice Guidance which restrict the use of planning obligations to secure affordable housing contributions from developments of 10 units or less. These changes were given legal effect following the Court of Appeal judgement in May 2016.
- 6.26 In view of this, and subsequent local appeal decisions which have afforded greater weight to the Written Ministerial Statement than the Council's adopted policy, the Council is not presently requiring financial contributions from applications such as this resulting in a net gain of 10 units or less. The absence of an agreed undertaking does not therefore warrant a reason for refusal in this case.

Community Infrastructure Levy (CIL)

- 6.27 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road,

public transport and community facilities which are needed to support new development. This development would be CIL liable and, although the exact amount would be determined and collected after the grant of planning permission, an informal calculation shows a CIL liability of around £125,853.55 with indexation applied.

## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date Received
Location Plan	LD03A/PL01		26.04.2018
Elevation Plan	LD03A/PL09		26.04.2018
Floor Plan	LD03A/PL06		26.04.2018
Floor Plan	LD03A/PL04		26.04.2018
Survey Plan	LD03A/PL02		26.04.2018
Floor Plan	LD03A/PL08		26.04.2018
Site Layout Plan	LD03A/PL03	A	07.06.2018
Block Plan	LD03A/PL11	A	07.06.2018
Street Scene	LD03A/PL14	A	07.06.2018
Street Scene	LD03A/PL15	A	07.06.2018
Elevation Plan	LD03A/PL05		03.05.2018
Elevation Plan	LD03A/PL07		03.05.2018
Floor Plan	LD03A/PL12		03.05.2018
Elevation Plan	LD03A/PL13		03.05.2018

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

4. No development shall take place until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Ho13.

5. No development shall commence including demolition and or groundworks preparation until all related arboricultural matters including tree protection measures, pre commencement meeting, arboricultural supervision and monitoring are implemented in accordance with the approved details contained in the Arboricultural Method Statement dated June 2018 and the Tree Plan numbered TPP03 dated June 2018 compiled by David Archer Associates.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies Pc4 and Ho9 of the Reigate and Banstead Borough Local Plan

Informative:

The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837

6. No development shall commence on site until a scheme for the landscaping and replacement tree planting of the site including the retention of existing landscape features has been submitted to and approved in writing by the local planning authority. Landscaping schemes shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or or use of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years

of planting shall be replaced within the next planting season by trees, shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies Pc4, and Ho9 of the Reigate and Banstead Borough Local Plan 2005.

Informative:

The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and shrubs shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.

7. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) provision of boundary hoarding behind any visibility zones
  - (f) measures to prevent the deposit of materials on the highway
  - (g) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

8. Plots 1 and 2 as shown on the approved site layout plan shall not be first occupied unless and until the proposed vehicular access to Dovers Green Road has been constructed in accordance with the approved plans.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

9. Plots 3, 4 and 5 as shown on the approved site layout plan shall not be first occupied unless and until the proposed vehicular access to Beehive Way has been constructed in accordance with the approved plans.



Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

10. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles at plots 1 and 2 to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking /turning areas shall be retained and maintained for their designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

11. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles at plots 3 - 5 to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking /turning areas shall be retained and maintained for their designated purposes.

Reason: The above conditions are required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users to satisfy policies Mo5 and Mo7 of the Reigate and Banstead Borough Local Plan 2005 and the objectives of the NPPF 2012.

12. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Pc4.

13. The first floor windows in the north and south side elevations of the development hereby permitted and other bathroom windows at first floor shall be glazed with obscured glass which shall be fixed shut, apart from a top hung opening fanlight whose cill height shall not be less than 1.7 metres above internal floor level, and shall be maintained as such at all times.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking with regard to Reigate and Banstead Borough Local Plan 2005 policy Ho9.

14. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer

windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason: To ensure that the development does not affect the amenity of the neighbouring property by overlooking and to protect the visual amenities of the area in accordance with Reigate and Banstead Borough Local Plan 2005 policy Ho9.

15. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A B and C of Part 1 of the Second Schedule of the 2015 Order shall be constructed.

Reason: To control any subsequent enlargements in the interests of the visual and residential amenities of the locality with regard to Reigate and Banstead Borough Local Plan 2005 policies Ho9, Ho13, Ho14 and Ho16

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The development hereby approved is liable to pay the Community Infrastructure Levy (CIL). Payment of CIL is mandatory. You are advised to familiarise yourself with CIL, its implications and your responsibilities. More information about CIL is available at [www.reigate-banstead.gov.uk/cil](http://www.reigate-banstead.gov.uk/cil). If you have not already done so, you should submit an Assumption of Liability Form as soon as possible to notify the Council who will be responsible for paying CIL for the development. This will ensure that the CIL Liability Notice, and any subsequent correspondence associated with CIL, is issued to the correct party. Responsibility to pay CIL will default to the landowner unless another party has assumed liability. All relevant forms can be found on the Planning Portal website at: <http://www.planningportal.co.uk/planning/applications/howtoapply/whattosubmit/cil>
4. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the

required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.

5. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
- (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowzers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

6. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.

7. The permission hereby granted shall not be construed as authority to carry out works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>.
8. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
9. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture / equipment.

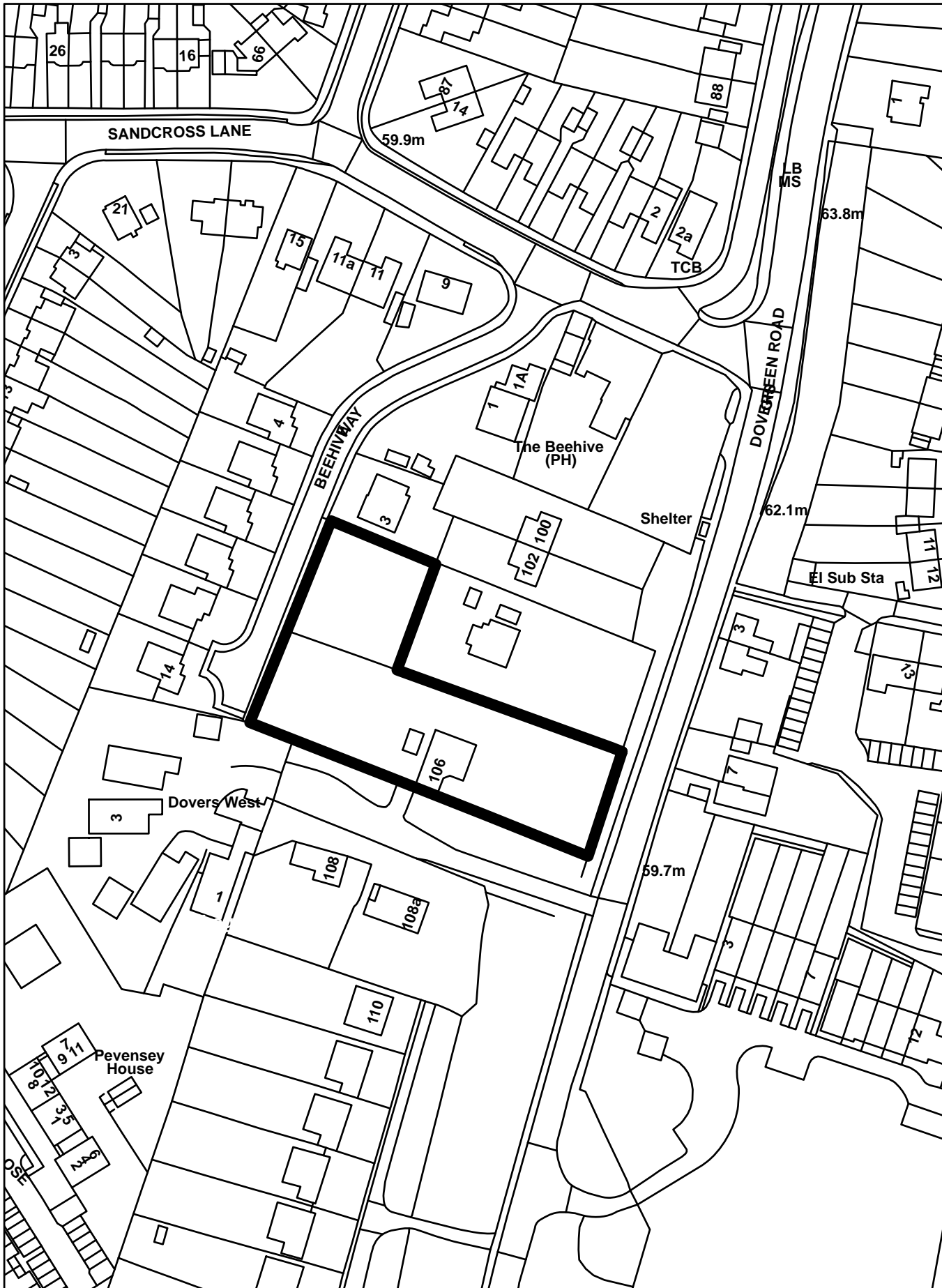
## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS4, CS8, CS10, CS11, CS14, CS15, CS17, Ho9, Ho13, Ho14, Ho16, Pc4, Mo5 and Mo7 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

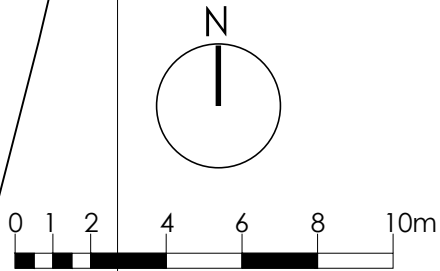
18/00916/F - 106 Dovers Green Road &  
Rear Of 104 Dovers Green Road, Reigate







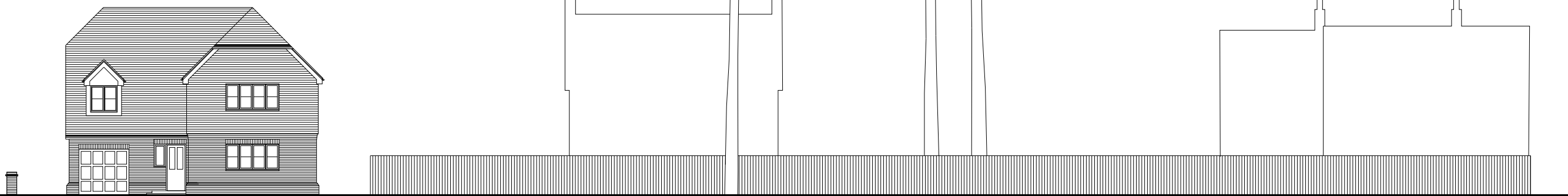
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Refer to Structural Engineers details for structural design criteria.  
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Rev	Date	Description
A	7.6.18	Layout amended after planning comments

Date	April 2018	Drawn	KA
Scale	1:200 @ A1	Checked	-
Client	Earlswood Homes		
Project	Dovers Green Road Reigate RH2 8DW		
Title	Proposed Site Layout		
Drawing No.	LD03A / PL 03 A		

Existing



Proposed



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Rev	Date	Description
A	7.6.18	Amended after planning comments



Date	May 2018	Drawn	KA
Scale	1:200 @ A3	Checked	*

Client  
Earlswood Homes

Project  
Dovers Green Road  
Reigate

Title  
Street Elevations  
Beehive Way

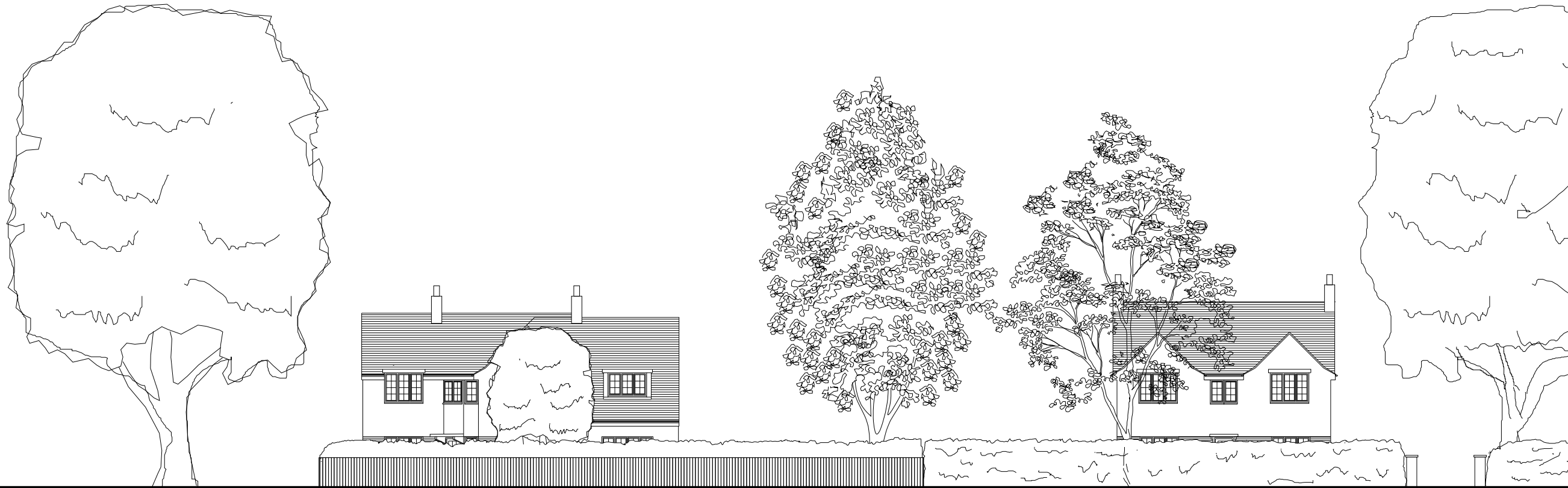
Drawing No  
LD03 A / PL 15 A





# Existing

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Dovers Way

No. 106

No. 104

# Proposed



Dovers Way

Plot 2

Plot 1

No. 104

Rev	Date	Description
A	7.6.18	Amended after planning comments



Date	May 2018	Drawn	KA
Scale	1:200 @ A3	Checked	*

Client  
Earlswood Homes

Project  
Dovers Green Road  
Reigate

Title  
Street Elevations  
Dovers Green Road

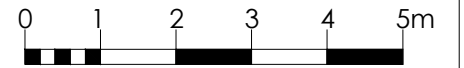
Drawing No

LD03 A / PL 14 A





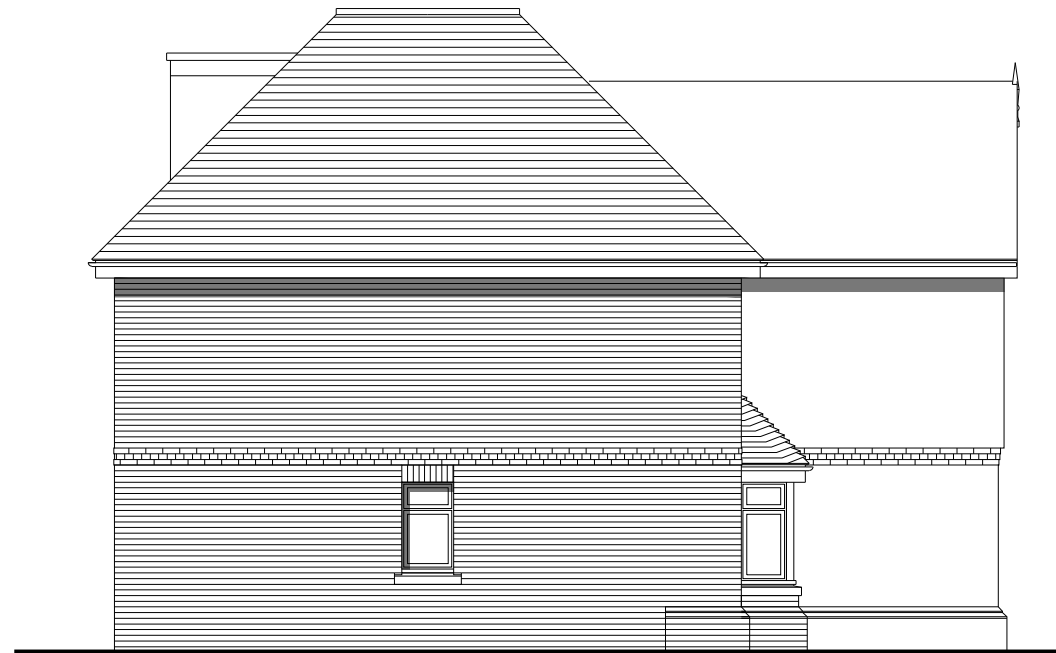
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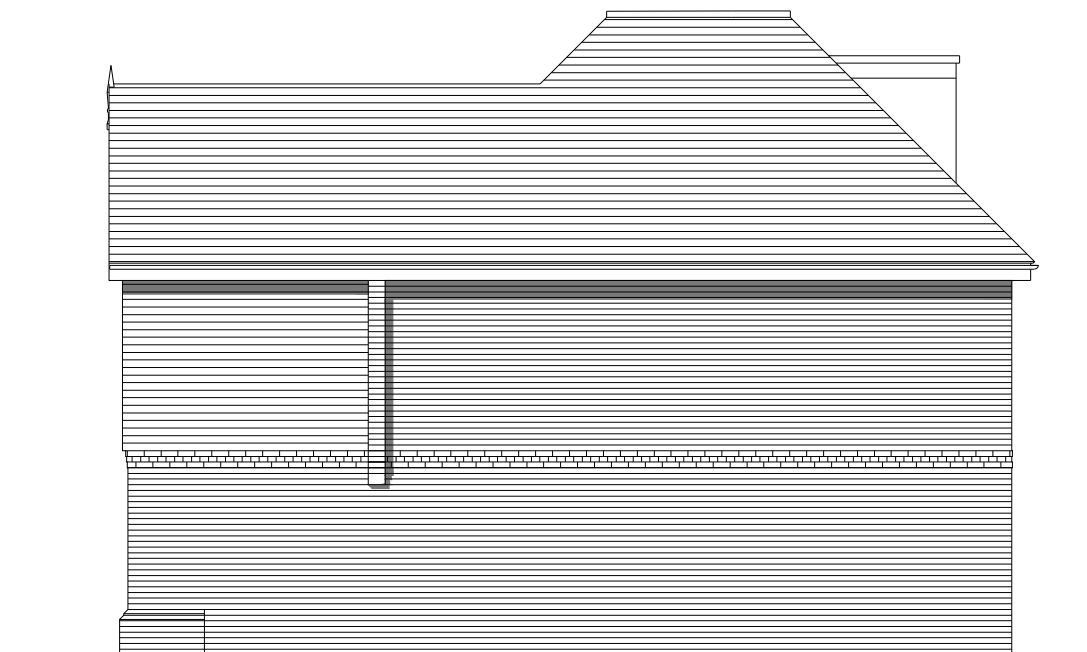
Front



Side



Side



Rear



Rev	Date	Description
		
Date	April 2018	Drawn KA
Scale	1:100 @ A3	Checked *
Client	Earlswood Homes	
Project	Dovers Green Road Reigate	
Title	Plot 4 Elevations	
Drawing No	LD03A / PL 07	



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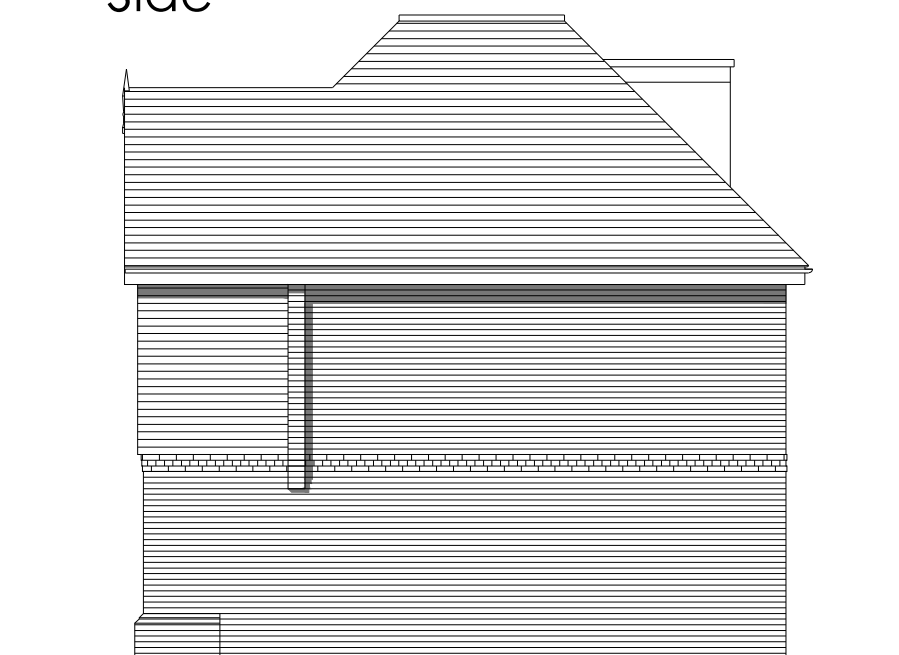
Front



Side




Side



Rear



Rev	Date	Description



**Earlswood**  
HOMES

Date April 2018	Drawn KA
Scale 1:100 @ A3	Checked *
Client Earlswood Homes	
Project Dovers Green Road Reigate	
Title Plot 5 Elevations	
Drawing No LD03A / PL 09	

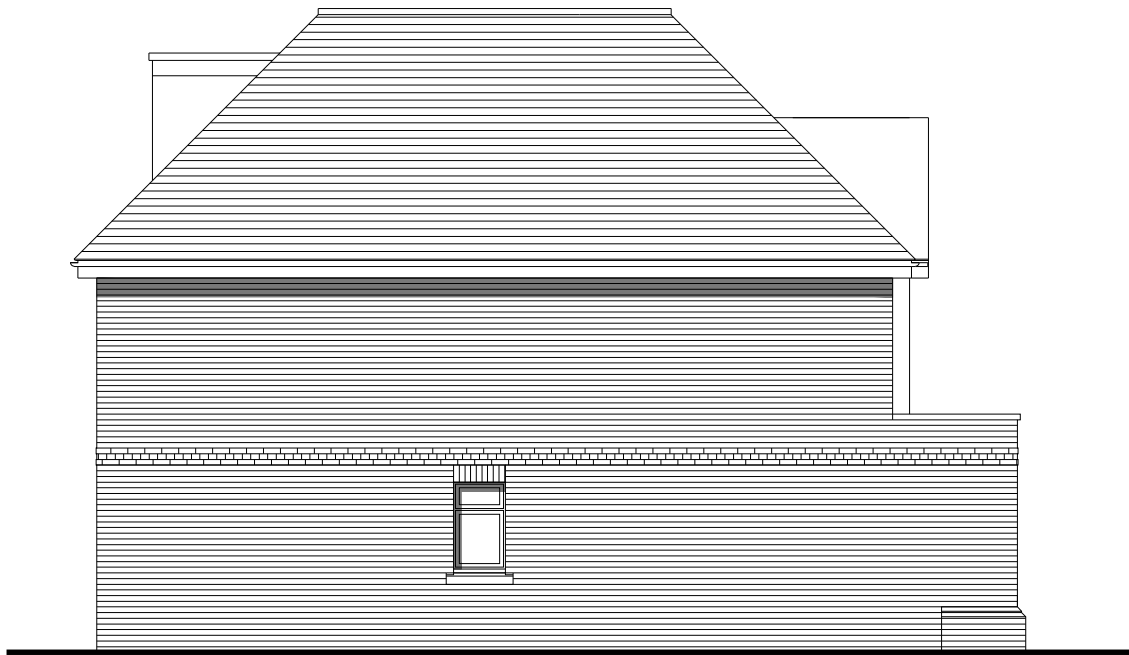
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Front



Side




Note: plots 1 & 3 as shown: plot 2 handed

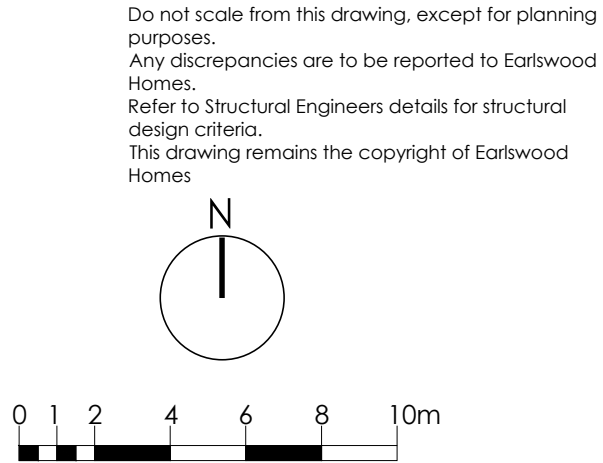
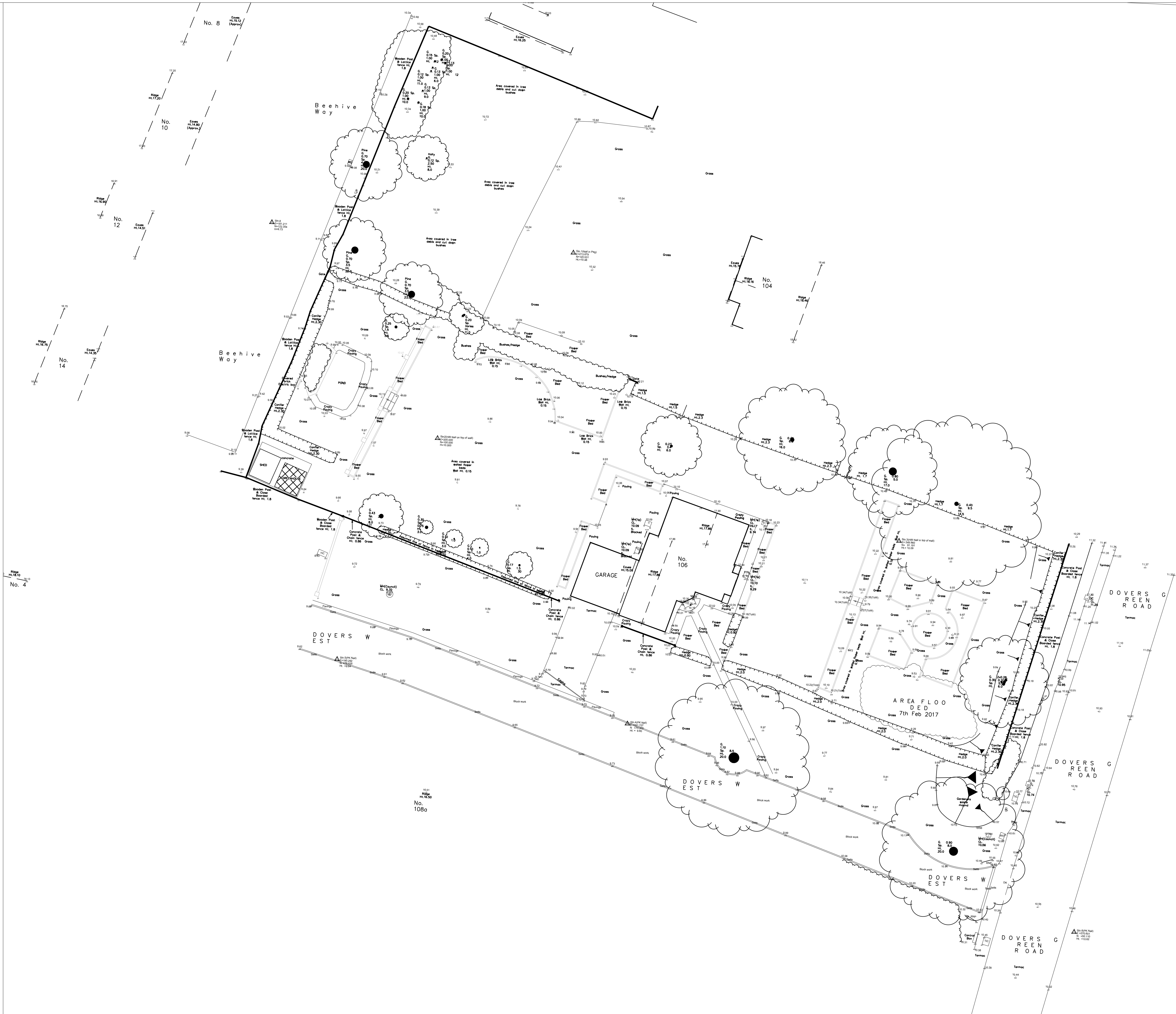
Side



Rear



Rev	Date	Description
		
Date	April 2018	Drawn KA
Scale	1:100 @ A3	Checked *
Client	Earlswood Homes	
Project	Dovers Green Road Reigate	
Title	Plots 1-3 Elevations	
Drawing No	LD03A / PL 05	



LEGEND	
BB	Ballard
BD	Belisha beacon
BS	Backdrop
BL	Bus stop
B.pier	Bed level
BPv	Brick pier
B/W	Brick paving
C/I	Brick wall
C/L	Corrugated iron
CL	Chain link
C/P	Cover level
Conc.	Concrete post
C/B	Concrete
CP	Close boarded
CR	Catch pit
CM	Cycle Rack
C/P & W	Cable marker
CPS	Conc. post & wire
DC	Conc. paving slabs
DK	Drainage channel
ELEC	Drop Kerb
EP	Electricity inspection cover
ER	Electricity pole
FB	Earthing Rod
FL	Flower bed
FH	Floor level
FLt	Fire hydrant
GP	Floodlight
CV	Gully
HYD	Gate post
Ht.	Gas valve
IC	Hydrant
IL	Height
IP	Inspection cover
IR	Invert level
Iwn	Iron post
JB	Iron railing
KO	Interwoven
LP	Junction Box
Mh(cp)	Kerb Outlet
Mh(ut)	Lamp post
Mh(fw)	Manhole - catch pit
Mh(sw)	Manhole - unable to lift
Mkr	Manhole - foul water
M/P	Manhole - storm water
PB	Marker
P/C	Metal post
P/W	Post
RC	Pillar box
RNP	Post & chain
R/W	Post & wire
RS	Road name plate
SBx	Retaining wall
SC	Road sign
SP	Salt Box
SPS	Stop cock
Stn.	Sign post
SV	Stone paving slabs
TCB	Survey control point
TIC	Stop valve
TLC	Telephone call box
TLCB	Telephone inspection cover
TP	Traffic light control
VP	Traffic light control box
VC	Telephone pole
WL	Vent pipe
WP	Valve chamber
WM	Water level
WMt	Wooden post
WV	Wire mesh
W/Pd	Water Meter
W/P & R	Water valve
	Wooden paling
	Wooden post & rail
Tree Annotation	
G	Diameter
S	Radial Spread
Ht	Height

**\*\*NOTE\*\***  
Co-ordinates and elevations are based on a local system not OS.

Rev	Date	Description

Date	April 2018	Drawn	KA
Scale	1:200 @ A1	Checked	-
Client	Earlswood Homes		
Project	Dovers Green Road Reigate RH2 8DW		
Title	Site Survey		
Drawing No.	LD03A / PL 02		